

SECTION 2

TRAFFIC CONTROL

2.1 GENERAL

This Section is intended to establish general principles of traffic control, worker protection and public safety measures to be taken in the performance of all work covered by these Improvement Standards.

No Specification contained herein shall be deemed to create a legal standard conduct or duty toward the public nor shall it limit the City in the exercise of powers conferred by law in modifying these Specifications under special conditions.

The requirements of the Manual of Uniform Traffic Control Devices (MUTCD), and California Supplement as most recently adopted by the State of California, herein referred to as the State Traffic Manual, shall take precedence over the requirements of this Safety Section.

2.2 TRAFFIC CONTROLS

The safe movement of traffic through construction areas depends upon communicating concise and proper information to the public by signs, barricades, delineators, flagmen and warning lights. All such devices necessary during construction shall be furnished by the Developer or City Contractor.

The size, shape and color of such devices as shown herein shall be as required by the State of California, Department of Transportation.

2.3 SIGNS

Warning signs used for night-time conditions shall be reflectorized or illuminated. The use of orange flags in conjunction with signs is permitted if they do not at any time interfere with a clear view of the sign face.

Reflectorized signs fastened to barricades or similar supports shall have the face of the sign vertical and normal to the direction of traffic for effective visibility.

Signs are to be used only as long as necessary and then removed. During periods when the signs are temporarily unnecessary, they shall be removed or have their message covered.

2.4 BARRICADES

Barricades are intended to impose an obstacle or close off, the normal flow of travel.

2.5 FLASHER SUPPORTS

Portable flasher supports shall be as required by the Standard Specifications.

2.6 DELINEATORS

The function of delineators is to channelize traffic. They shall consist of post and paddle type markers or cylindrical or cone shaped objects 18 to 48 inches in height.

Delineators should be uniformly positioned laterally and longitudinally relative to the line of traffic and they must be maintained in an erect position.

Delineators for night use shall be reflectorized or illuminated to be visible from 500 feet under normal conditions.

When placed in close proximity to the edge of a traffic lane, delineators shall be made of a material that will withstand impact without damage to them or the striking vehicle.

Consideration must also be given to the necessity for stability against knockdown from wind or from the wash of passing traffic.

2.7 FLAGMEN

A flagman is one of the oldest and most basic means of controlling traffic. He/she can observe changing conditions and transmit information to the motorist based on current conditions. He/she can also act as a guard in advance of a work party by observing approaching traffic and being prepared to warn the workmen. A flagman should be used only when such discretionary capability is required, and not as a substitute for other warning signs and devices.

When a flagman is necessary, he must convey a message and the message must be timely and accurate. His/her effectiveness and the safety of the traffic and his/her fellow workmen depend upon the way he/she works. Standard hand signals shall be used as shown in the State Traffic Manual.

2.8 WARNING LIGHTS

Warning lights shall be electric lanterns, electric markers or flashers provided to indicate an obstruction or restriction during periods of low visibility. Warning lights shall be placed to mark the location of obstructions. Motion may be imparted to warning lights.

Warning lights may be fastened to signs, barricades and portable flasher supports in a manner satisfactory to the Engineer.

2.9 STREET CLOSURE

During the period of construction, the Developer or City Contractor shall maintain the public way so as to not unreasonably hinder, render inconvenient or interfere with be public use thereof and shall erect such barriers, signs and other measures as may be necessary and proper to provide for the safety of persons, animals and vehicles using the public way. The City Public Works Director or designated representative shall be the sole judge of the interpretation of the provisions of this paragraph, and all decisions rendered regarding same shall be final. When safety measures are required, all construction shall immediately cease until such measures are provided to the satisfaction of the City Public Works Director or designated representative.

When partial temporary street closures are made, one lane for each direction of through traffic must be maintained except where flagmen are provided to control traffic, then one lane may serve both directions. When trenching is necessary across intersecting streets, the work shall be done in such a manner as to maintain two-way traffic on cross streets at all times.

Where trench line crosses an entrance to private property, access to the property shall be maintained at all times by means of a suitable bridge, until the trench may be backfilled. Such bridges shall be property guarded, barricaded and illuminated at night. Where any crosswalk is cut by the trench, suitable bridging shall be constructed. Such bridging shall be at least 4 feet in width, shall have suitable hand railing, shall be properly guarded and illuminated at night.

The complete closure of a street is allowed only when authorized by the City. Such closure shall be conditioned by the City.

2.10 RULES AND REGULATIONS

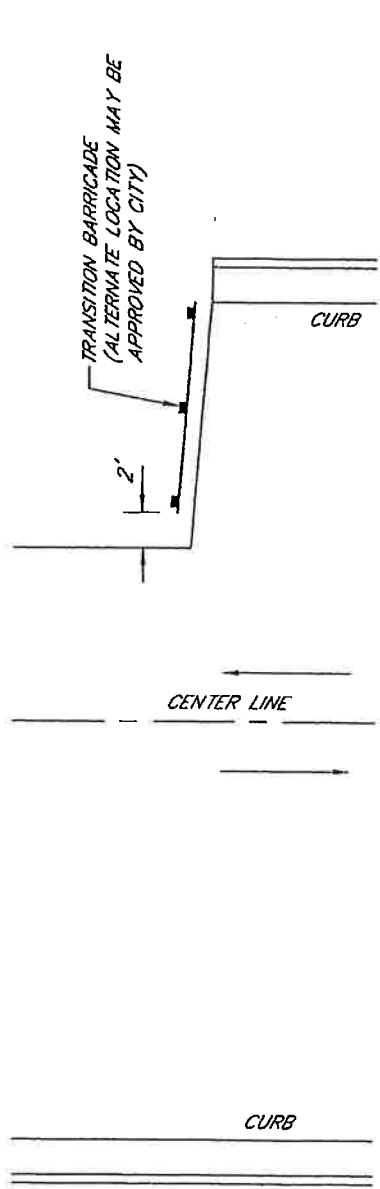
All work performed and all materials used by the Developer or City Contractor shall comply with the following: State Labor Code, California Administrative Code, Construction Safety Order, Title 8, Subchapter 4; and all other applicable Federal, State and local laws and regulations.

Specifically, the Developer or City Contractor shall furnish, install and maintain all shoring, bracing and sheeting, bracing or sheeting shall be repaired at the Developer's or City Contractor's expense.

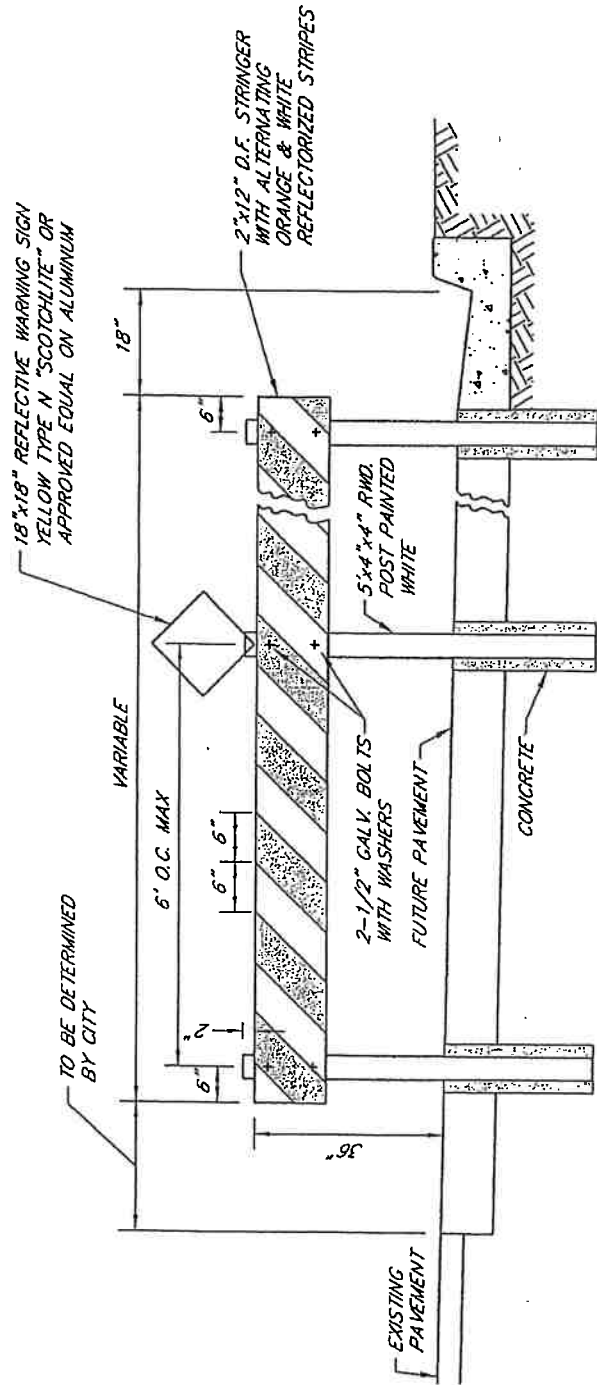
All excavation shall be in compliance with Section 1540(a) (1) of the Construction Safety Orders (Title 8, California Administrative Code Section 1540), which states:

1. "Prior to opening an excavation, effort shall be made to determine whether underground installations; i.e., sewer, water, fuel, electric lines, etc., will be encountered and, if so, where such underground installations are located. When the excavation approaches the approximate location of such an installation, careful probing or hand digging shall determine the exact location; and when it is uncovered, adequate protection shall be provided for the existing installation. All known owners of underground facilities in the area concerned shall be advised of proposed work at least 48 hours prior to the start of actual excavation."

THE DEVELOPER AND/OR CONTRACTOR SHALL BE RESPONSIBLE FOR PUBLIC SAFETY, THE PRESERVATION OF, AND ANY DAMAGE TO, BOTH PRIVATE AND PUBLIC PROPERTY, AND JOB SITE SAFETY IN ACCORDANCE WITH THE CURRENT STANDARD SPECIFICATIONS. SPECIAL ATTENTION IS DIRECTED TO SECTIONS 7-1.09, 7-1.11 AND 7-1.12 OF THE STATE STANDARD SPECIFICATIONS.



PLAN VIEW



TYPICAL BARRICADE WHERE PAVEMENT NARROWS

ALIGNMENT TO BE DETERMINED BY CITY



PARTIAL STREET BARRICADE

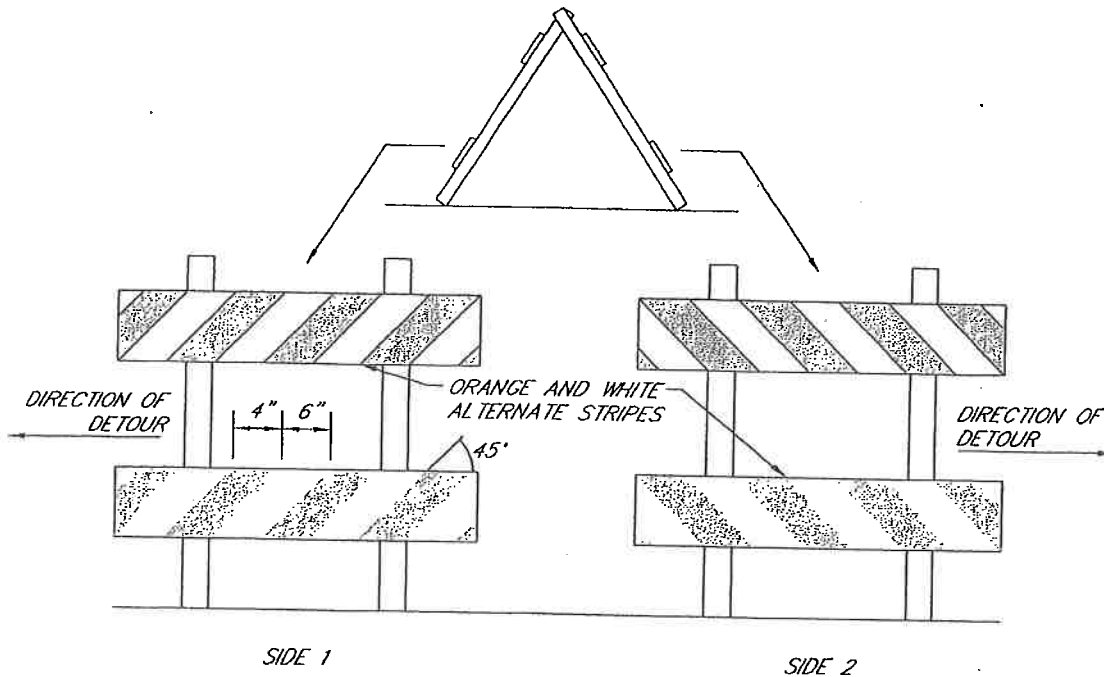
DRAWN BY: A.D.R.
 CHECKED BY: R.H.H.
 SCALE: NONE
 DATE: 1/04

APPROVED BY: *[Signature]*
 DATE APPROVED: 5/1/04

CITY OF HUGHSON

STANDARD DETAIL

2-TR.1



BARRICADE USAGE (CLASS I OR CLASS II)

1. WHERE CLASS I BARRICADES EXTEND ENTIRELY ACROSS A ROADWAY, IT IS DESIRABLE THAT THE STRIPES SLOPE DOWNWARD IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN IN DETOURING.
2. WHERE BARRICADES ARE USED TO MARK OBSTRUCTION OR TO GUIDE TRAFFIC THROUGH A WORK AREA, THE STRIPES SHOULD SLOPE DOWNWARD TOWARD THE SIDE WHERE TRAFFIC SHOULD PASS.

TYPICAL CLASS II BARRICADE



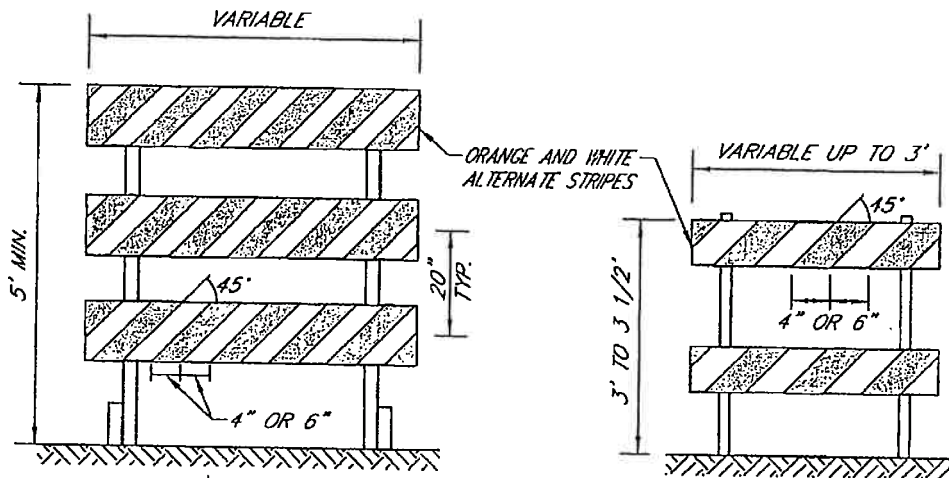
TYPICAL CLASS II BARRICADE

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 SCALE: NONE
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 DATE APPROVED: 5/1/04

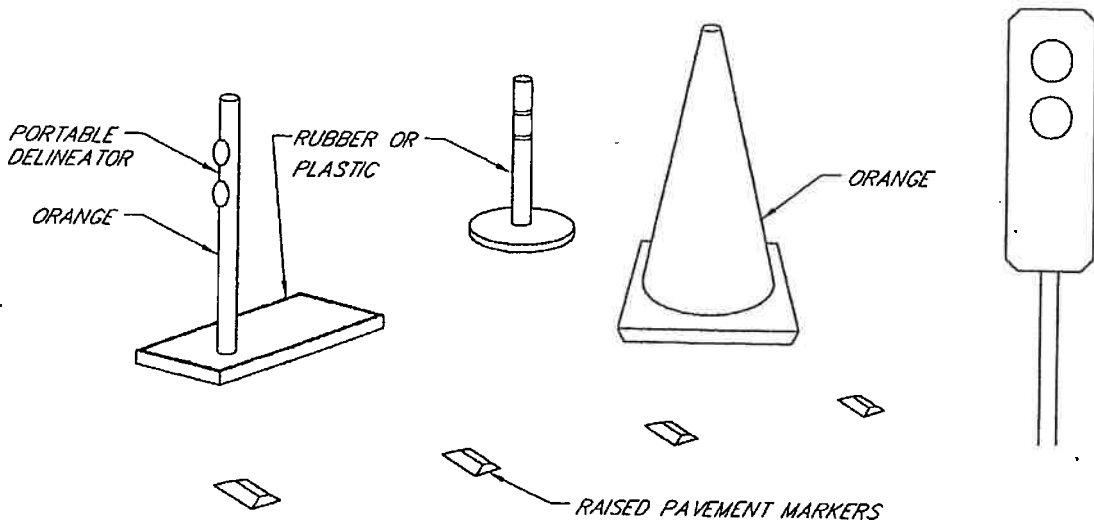
CITY OF HUGHSON

STANDARD DETAIL
2-TR.2



BARRICADE CHARACTERISTICS

CLASS	I	II
WIDTH OF RAIL	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL	6' MIN. - 8' MAX.	3' MIN. - 4' MAX.
WIDTH OF STRIPES	6"	6"
HEIGHT	3' MIN.	3' MIN.
TYPE OF FRAME	DEMOUNTABLE OR HEAVY "A" FRAME	LIGHT "A" FRAME
FLEXIBILITY	ESSENTIALLY MOVABLE	PORTABLE



BARRICADES & DELINEATORS

DRAWN BY: A.D.R.
 CHECKED BY: R.H.H.
 SCALE: NONE
 DATE: 1/04

APPROVED BY: *[Signature]*
 DATE APPROVED: 5/1/04

CITY OF HUGHSON

STANDARD DETAIL

2-TR.3